

## OLD TOWN AND OLD TOWN WEST ZONING DISTRICT

### TEST LEVEL 1 DESIGN GUIDELINES FOR THE OLD TOWN DISTRICT

#### SITE DESIGN ISSUES

##### *Lot size*

1. **The minimum buildable lot size in the Old Town District is 2,500 square feet, although smaller lots may be considered under special review.**
  - These limits are defined in the town's zoning ordinance.
  - Where lots are to be less than the minimum, the design guidelines that follow must still be met.

##### *Lot coverage*

2. **No lot coverage restrictions are provided in the Old Town District.**
  - Portions of the site that are not covered by buildings shall be landscaped, except for service areas. (See guideline 144.)

##### *Landscaping*

(Also see landscaping guidelines on page 57.)

3. **New development that abuts a residential zone shall provide a landscape strip along the common property line.**
  - The landscape strip shall be a minimum of five feet in width.
  - A mixture of evergreen and deciduous plants, trees, shrubs and ground covers shall be provided, such that a year-round effect is accomplished.

Portions of the site that are not covered by buildings shall be landscaped, except for service areas.

4. **A minimum of 5% of the property shall be landscaped, as defined in the zoning ordinance.**
  - All new construction and major rehabilitation projects shall provide, the 5% landscaped area.
  - Landscaping shall be located such that it is visible from the public way.

New development that abuts a residential zone shall provide a landscape strip along the common property line.

### *Storm Drainage*

5. **Design drainage areas such that they will also serve as site amenities.**
  - Landscape storm water detention areas, for example.
6. **Design site drainage such that it does not impact adjoining properties.**
  - Drain surface water to collection points such that it can be introduced into a municipal storm sewer system.

## **BUILDING DESIGN ISSUES**

### *Setbacks*

*No setbacks are required in the Old Town District, and generally, storefronts are expected to align at the sidewalk edge. There are occasions, however, where a primary facade may be set back from the walkway. In such cases, other pedestrian-oriented amenities should be provided in these areas. A greater variety in rear yard set-backs is also expected, because service areas and parking spaces are more likely to be located along alleys.*

7. **Aligning storefronts at the sidewalk edge is encouraged, however, a moderate variety in setbacks is also appropriate in this area.**
  - If some setback from the sidewalk is necessary, it should be developed as an area of pedestrian activity, such as a court yard or plaza.
  - Front setbacks should not be used for parking.
8. **In general, a building should be built to the full width of the lot as it faces the street.**
  - Exceptions are where plazas or other actively used outdoor areas are located along the side of buildings.

*Moderate building setback with plaza.*

*Building the full width of a lot.*

### *Building height*

9. **New structures and additions should reinforce the established range of building heights in the Old Town District.**
  - Buildings should maintain the sense of one and two-story building heights as perceived at the sidewalk edge.
  - Use horizontal elements, such as cornices and moldings, to express the typical one and two story heights at the sidewalk edge. Taller portions of buildings may be set back from the front elevation.
  - Buildings may be taller than two stories, but should have elements that express these traditional heights.

- Align storefront elements with others on the street, where feasible, to enhance the sense of scale of the street.

*Buildings should maintain the sense of one and two story building heights.*

*Taller portions of buildings may be set back from the front elevation.*

#### *Scale and proportion*

- 10. Buildings in the Old Town District should have a sense of pedestrian scale.**
  - Use small scale elements at the ground level to provide interest for pedestrians. Use materials that occur in modules that have recognizable dimensions that are familiar to pedestrians such that they can determine the scale of the building.
  - Use windows and doors in sizes seen traditionally, such that the viewer can relate to the scale of these elements.
- 11. Large buildings should have horizontal emphasis in their overall proportions.**
  - Use horizontal moldings, marquees and canopies, located at heights seen traditionally, to establish horizontal emphasis.
- 12. Primary facades should express dimensions of typical storefront widths traditionally found on the street.**
  - Most buildings have a facade width of 25 feet, or multiples of this dimension.
  - New buildings should have facade elements that express these traditional proportions, and thereby reinforce this established rhythm. For example, vertical trim boards placed every 25 feet would express this traditional module.

*Buildings should have a sense of pedestrian scale.*

*Primary facades should express dimensions of typical store front widths traditionally found on the street.*

## **TEST LEVEL 2 DESIGN GUIDELINES FOR THE OLD TOWN AND OLD TOWN WEST DISTRICT**

### **SITE DESIGN ISSUES**

#### *Directional expression and building orientation*

- 13. Buildings should appear to "address" the street.**
  - Primary entrances and display windows should be oriented to the sidewalk.
- 14. Blank walls along major sidewalks are inappropriate.**
  - Building faces should provide visual interest; include display windows, decorative patterns and landscaping to emphasize an orientation to the sidewalk.

Primary entrances should be oriented to the sidewalk.

#### *Parking location*

*As much as it is feasible, the visual impact of parking should be minimized.*  
(Also see parking guidelines.)

**15. Locate parking areas to the side or rear of buildings.**

- Parking lots should not be located along the edge of Pacific Avenue.
- Note that new curb cuts are not allowed along Pacific Avenue.

Locate parking to the side or rear of buildings.

### **BUILDING DESIGN ISSUES**

#### *Roof materials*

**16. Where roofs are visible, black composite roofing shingles are encouraged.**

- Tan or earth-tone colored composition shingles are also allowed.
- Wood shingles and file (again, in muted earth tones) are also appropriate.

#### *Siding Materials*

**17. Wood shall be the primary exterior siding material in the Old Town District.**

- Natural wood siding, such as board and batten, horizontal clapboard, shiplap, or wood shingle, applied to the building exterior, are appropriate materials.
- Shingles are especially encouraged in the Old Town District.
- Matte or semi-gloss finishes are appropriate; shiny surfaces are to be avoided for primary exterior siding materials because they are Out of character with early seashore designs.
- Synthetic materials are inappropriate for use as siding.

#### *Doors*

Wood shall be the primary exterior siding material in the Old Town District.

**18. Wood doors are preferred.**

- Doors that are similar in character to wood doors may also be considered.
- Primary entry doors should have an upper panel of glass because these are similar to those used historically and they provide views into building interiors, thereby enhancing interest to pedestrians.

**20. Primary entrances shall be located in recessed entries.**

- This provides shelter for pedestrians and also adds interest to the street.

Primary entrances shall be located in recessed entries.

## *Windows*

- 21. All properties should include traditional storefront display windows at the sidewalk.**
  - These provide visual interest to pedestrians and enhance the pedestrian-oriented environment.
- 22. Wood windows are preferred.**
  - If synthetic materials are to be used, they must have wood trim, or casing, to "tie" them in with the wood siding.
  - The window sash must have a matte or semi-gloss painted finish.
- 23. Windows with vertical proportions are preferred on upper stories.**
  - These may be combined in sets to create larger areas of glass.
  - When repeated along the street these windows create a rhythm that contributes to a sense of visual continuity for the street.

## *False fronts*

- 24. False fronts are appropriate for building elevations that face the street.**
  - Actual roof forms that are concealed behind the false fronts may be sloping or flat.

*Traditional storefront display.*

*False fronts are appropriate for building elevations that face the street.*

## *Marquees*

- 25. Canopies and marquees are encouraged because they provide protection from the elements, and promote foot traffic in inclement weather.**
  - Marquees, which are horizontal canopies that project from building facades, are especially useful in providing a sense of scale and protection for pedestrians.
  - The use of recessed lighting is encouraged. Flashing lights are inappropriate.

*Awnings may also be considered.*

*Canopies and marquees are encouraged.*

## TEST LEVEL 3 DESIGN GUIDELINES FOR THE OLD TOWN AND OLD TOWN WEST DISTRICT

### SITE DESIGN ISSUES

#### *Parking arrangement*

(Also see parking guidelines on page 52.)

**26. Minimize the visual impacts of parking lots within the Old Town District.**

- Divide large parking lots into smaller areas, which are separated by landscaping.
- Screen parking areas from view with earth forms, and other landscaping.
- The use of porous materials in parking lots is encouraged, because this will minimize storm run-off.
- Smooth, hard surfaces, such as concrete, are of course expected where accessibility by persons with mobility impairments is needed.

*Divide large parking areas into smaller areas which are separated by landscaping.*

#### *Landscaping*

(also see landscaping guidelines on page 48.)

**27. Define site edges with landscaping.**

- Use planting areas, fences and hedges to define property lines.
- This will provide interest to pedestrians.

#### *Screening of amusement sites*

**28. Provide landscaping along the edges of amusement sites from the public way.**

- Use fences, hedges and landscape devices to define the edges of sites along public sidewalks.
- This does not mean, however, that these sites must be visible from the street.

#### *Plant materials*

**29. Enhance the sense of visual continuity in the Old Town District by using similar plant materials in landscape schemes.**

- Property owners are encouraged to use plant materials similar to those used on other properties in the zone, in order to achieve a sense of visual continuity.

#### *Site decorations*

*Some commercial sites include the use of special decorative elements, such as outdoor sculpture. Many of these features have nautical themes, such as buoys and floats. These elements are allowed, but should be subordinate to the overall project design.*

**30. Locate site decorations within the context of landscaped areas.**

- Site decorations should accurately convey the heritage of the community.
- Symbols and product forms that convey the nature of the business conducted are also appropriate as site decorations.

*Use planting areas, fences and hedges to define property lines.*

*Locate site decorations within the context of landscaped areas.*

#### *Utilities*

- 31. Locating utilities lines underground is encouraged.**
- 32. The use of decorative street lights is encouraged.**
- 33. The use of decorative sidewalk paving is encouraged.**
- 34. Service areas for public utility boxes shall be screened, where feasible.**

### **BUILDING DESIGN ISSUES**

#### *Fences*

- 35. The use of fences is encouraged to define the edges of open areas.**
  - Decorative fences are preferred, particularly wooden picket fences.
  - Painted metal fences, which are simple in character, also may be considered.
  - Chain link and split rail wood fences are not encouraged.

*Wood picket fences are encouraged.*

#### *Expression of detailing*

- 36. The use of ornamental detail is encouraged.**
  - Ornamental detail should not convey a false sense of history for the area, but modern interpretations of historic details are encouraged.
  - Details should be located such that they provide visual interest to pedestrians. False front cornices, roof gables, eaves and marquees are appropriate locations. Detailing is also appropriate around doors and windows.
- 37. Ornamental detail should be simple in design, in keeping with the character of details used historically in Long Beach.**
  - Ornamental detail should appear to be integrated into the overall architectural composition.

## *Color*

### **38. Muted colors are appropriate for building exteriors.**

- Bright colors that contrast strongly with the natural colors of the area are inappropriate.
- Matte or semi-gloss finishes are appropriate for building exteriors. Avoid high gloss finishes.

### **39. Minimize the number of colors used in a color scheme.**

- Keep color schemes simple. This will enhance the overall sense of composition of the building.
- Designs using two or three colors are encouraged.

*The use of ornamental detail is encouraged.*

## *Mechanical equipment and service areas*

### **40. All ground-mounted mechanical equipment and service areas shall be screened from view.**

- Fences, berms and plantings are appropriate screening methods.

*Screen ground mounted mechanical equipment.*

## **SIGNS**

### *Character of signs*

### **41. Signs that are simple in design, and that are easily read are encouraged.**

- Signs that use pictographics to convey the activity of the business are particularly encouraged.

### *Size of signs*

### **42. The size of a sign shall not exceed the limits defined in the zoning ordinance.**

- The total sign area for a single business property will not exceed 50 square feet. For multiple tenant buildings, the total allowable signage may not exceed two square feet per linear foot of street frontage, up to a maximum of 150 square feet per building.
- Each multiple-building complex is permitted one freestanding sign, not to exceed one square foot per linear foot of street frontage, up to a maximum of 75 square feet.
- Businesses within the complex shall be permitted a wall sign, not to exceed one square foot per linear foot of wall frontage.



*A sign should obscure details. appear integrated into its setting. It should not obscure details.*

#### *Sign location*

**43. A sign should appear to be integrated into its setting.**

- If mounted on a building, a sign should appear to be a part of architecture and fit within architectural features.
- If mounted on the ground, a sign should appear to be an integral part of the landscaping.
- A freestanding sign should have a low profile.

**44. A sign should appear to be in proportion to its building.**

#### *Sign materials*

**45. Signs shall have materials and colors that are similar to those of the building.**

- Painted wood signs are encouraged.

#### *Illumination of signs*

**46. Signs may only be lighted by concealed, indirect lighting.**

- No flashing lights, gas-filled or internally lit sources are permitted.

*A sign should appear to be in proportion to its building.*

*Signs that use pictographics are encouraged.*

### **OTHER DESIGN ISSUES**

#### *Cohesiveness*

**47. The design of a building and its site shall have a sense of integratedness.**

- A sense of cohesiveness should be achieved for the entire project.

**48. A sense of being visually related to other Old Town theme buildings also should be achieved.**

- Repeating similar forms, materials, colors, landscaping, and building scales, are methods of achieving this sense of relatedness.
- This does not mean that new designs must copy historic styles.
- Also note that some design may achieve a sense of relatedness, and meet the spirit of the guidelines, even though some details are different from those promoted in specific guidelines.

## SIGNS

### *Character of signs*

**49. Signs that are simple in design, and that are easily read are encouraged.**

- Signs that use pictographics to convey the activity of the business are particularly encouraged.

*Low scale monument signs; integrated into bermed earth forms, are encouraged.*

*In some areas where portions of the site are given over to parking lots, freestanding signs are used. Although pole mounted signs are allowed, other forms more closely integrated with the site should be used.*

**50. Low scale monument signs, integrated into bermed earth forms, are encouraged.**

- Use directory signs for multiple businesses in one location.

*Use directory signs for multiple businesses in one location.*

*Where several businesses are in the same building, use similar sign positions and shapes to minimize visual confusion.*

**51. Avoid a wide variety of sign shapes and sizes.**

**52. Align signs, and use similar shapes to improve legibility.**

### *Size of signs*

**53. Each single business property shall be permitted a total sign area not to exceed one-hundred (100) square feet. A freestanding sign may not exceed twenty-five (25) square feet of the total allowable signage.**

### *Commercial District Design Guidelines*

**54. Each multiple-tenant building is permitted one freestanding sign not to exceed fifty (50) square feet of the total allowable signage. Total allowable signage may not exceed two square feet per lineal foot of street frontage up to a maximum of one hundred and seventy-five (175) square feet.**

**55. Each multiple-tenant building complex is permitted one free-standing sign not to exceed one square foot per lineal foot of street frontage up to a maximum of one-hundred (100) square feet. Each business within the complex shall be permitted a wall sign not to exceed one square foot per lineal foot of wall frontage.**

### *Sign Location*

**56. A sign should appear to be integrated into its setting.**

- If mounted on a building, a sign should appear to be a part of the architecture and fit within architectural features.
- If mounted on the ground, a sign should appear to be an integral part of the landscaping.
- A freestanding sign should have a low profile.

### *Sign materials*

**57. Signs shall have materials and colors that are similar to those of the building.**

### *Illumination of signs*

## **COMMERCIAL DISTRICT**

### **TEST LEVEL I DESIGN GUIDELINES OR THE COMMERCIAL (CI) DISTRICT**

#### **SITE DESIGN ISSUES**

##### *Lot Size*

- 58. The minimum lot size shall comply with the zoning ordinance.**
- The minimum lot size shall be 5,000 square feet. Construction on lots platted prior to the passage of the zoning ordinance, with less than 5,000 square feet, will be permitted as approved by the City Council upon the recommendation of the Planning Commission.

##### *Lot coverage*

- 59. No lot coverage restrictions are provided in the Commercial District.**

##### *Storm Drainage*

- 60. Design site drainage such that it does not impact adjoining properties.**
- Drain surface water to collection points such that it can be introduced into a municipal storm sewer system.

#### **BUILDING DESIGN ISSUES**

##### *Building Heights*

- 61. The maximum height of a structure shall be 35 feet, measured as the vertical distance from the average grade to the highest point of the roof.**

### **TEST LEVEL 2 DESIGN GUIDELINES FOR THE COMMERCIAL (CI) DISTRICT**

#### **SITE DESIGN ISSUES**

##### *Orientation*

- 62. Orient the building to the street where possible.**
- Place the building forward and parking to the rear or side, where feasible.

### *Site design*

*Because large surfaces in the Commercial District are given over to auto service and parking, open spaces are major portions of the visual field. Therefore, enhancing these spaces is important.*

**63. Use landscaping elements to screen edges of open sites.**

- **Maintain the sidewalk edge with fences, railings and landscaping. Where buildings are at the sidewalk edge, maintain this relationship.**

*Landscaping within the site is as important as along the edges.*

**7 Accent paving is encouraged to visually link a parking lot to a building and to identify pedestrian ways.**

*Orient the building to the street where possible. Place the building forward and parking to the rear or side.*

*Accent paving is encouraged to visually link a parking lot to a building and to identify pedestrian ways. Landscaping is used to maintain the sidewalk edge.*

*Extensive amounts of grey paving surfaces discourage pedestrian activity and weaken the edge distinction between the site and the street. Curb cuts for driveways can contribute to this problem.*

**64. Avoid multiple curb cuts. These complicate turning movements and disrupt the sidewalk**

**65. Minimize curb cuts by combining driveways within adjacent properties, where feasible.**

### *Parking Location*

**66. Locate off-street parking to the rear of the site, if feasible.**

- Keep paved areas and curb cuts for driveways to a minimum.
- Use plantings to screen parking areas.

*Minimize curb cuts by combining driveways within adjacent properties.*

**67. Plan large parking lots to be subdivided into small components so that the visual impact of large paved areas is reduced.**



- Provide planting buffers at the edges of parking lots. Or use decorative paving to define the site border. Also include islands to planting in the interior of lots.
- Side or rear locations are preferred for parking lots.

*Arrangement of parking areas*

- 68. Break up large parking areas with landscaping and accent paving. A minimum of 10% of the area should be landscaped.**

*The edge of the parking lot at the sidewalk is important to consider, because it is seen at close range by both pedestrians and motorists.*

- 69. Landscape edges of parking lots with trees and shrubs.**
- 70. Use streetscape items at the sidewalk edge when possible.**



**A bank with landscaping between the building and sidewalk offers a unique setting.**

*Pedestrian orientation*

*Sidewalks should be enhanced with amenities to encourage pedestrian circulation. There are opportunities to develop special pedestrian ways within the interior of large private parking lots.*

- 71. Where feasible, define and enhance pedestrian routes with landscaping.**
- 72. Consider using accent paving and plant materials to define pedestrian routes.**

*Landscape edges of parking lots with trees and shrubs.*

## **BUILDING DESIGN ISSUES**

### *Building form*

*A balance between landscaping and building is an objective in the Commercial District. Although the details of facade design are less critical in this district, the overall form of each building should complement the streetscape.*

- 73. Develop simple geometric building forms that can contribute to a sense of visual continuity on the street. Avoid exotic forms that may visually fragment the streetscape.**
- 74. Keep the storefront visible from the parking lot and oriented to the pedestrian.**
- 75. Use typical storefront elements to enhance pedestrian appeal from the street, such as display windows and awnings facing the street.**
- 76. Use accent paving to identify entrances from the parking lot to building entrance.**
- 77. At grade entrances, facing the street, are encouraged.**

*Use typical storefront elements to enhance pedestrian appeal from the street, such as display windows and awnings. Where feasible, orient the building to the street and place parking to the rear or side of the building.*

### *Building materials*

- 78. A wide range of building materials are acceptable, but muted tones are encouraged for the primary materials although variety is anticipated. Natural materials (including wood and stone) are encouraged.**

## **TEST LEVEL 3 DESIGN GUIDELINES FOR THE COMMERCIAL (CI) DISTRICT**

## **BUILDING DESIGN ISSUES**

### *Color*

- 79. Muted Colors are appropriate for building exteriors.**
  - Bright colors that contrast strongly with the natural colors of the area are inappropriate.
  - Matte or semi-gloss finishes are appropriate for building exteriors. Avoid high gloss finishes.



**80. Minimize the number of colors used in a color scheme.**

- Keep color schemes simple. This will enhance the overall sense of composition of the building.
- Designs using two or three colors are encouraged.

*Mechanical equipment and service areas*

**81. Minimize the visual impact of mechanical equipment.**

- Enclose mechanical equipment in structures or provide a screen where feasible.
- Locate antennas and satellite dishes such that their visibility from the street will be minimized.

*Minimize the visual impact of mechanical equipment and service areas.*

# TEST GUIDELINES FOR ZONING DISTRICTS

## SHORELINE RESORT AND SHORELINE RESORT RESTRICTED

### TEST LEVEL 1 GUIDELINES FOR THE SHORELINE RESORT DISTRICT

#### SITE DESIGN ISSUES

##### *Siting*

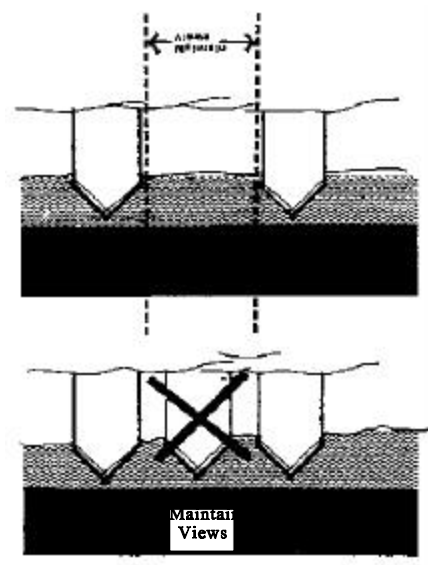
**82. Locate buildings such that they appear to be sited in the sand dunes.**

- Avoid filling dune areas to create a larger building site. (Establishing a building elevation above flood levels is of course expected, however.)
- Also encouraged are berms and other sloping land forms against buildings to reduce the perceived building height.
- Retain the existing topography, where feasible, and minimize filling.
- Also establish a natural transition in site grades to neighboring properties.

##### *Position on site; view protection and enhancement*

**83. Locate a building on its site such that views through the development are retained and open space is maximized.**

- Views from the public way, including those
- from streets, and the beach in particular,
- should be preserved, where feasible.
- Locate buildings on the site such that side
- and rear yards may be combined visually
- with adjacent properties in order to increase
- the visual impact of open space.

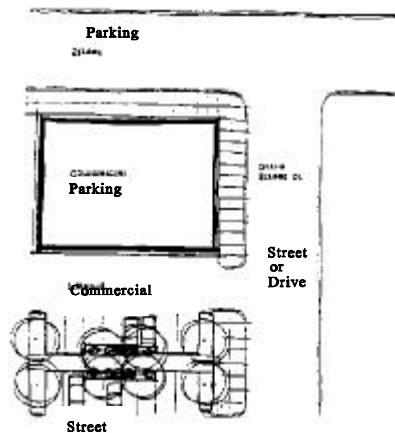


##### *Location of parking areas*

(Also see parking guidelines on page xx.)

*The visual impacts of parking areas should be minimized throughout the neighborhood. The intent is to minimize the amount of paved hard surfaces in development, to retain a natural image and reduce storm run-off.*

**84. Parking should be located to the rear or interior of lots wherever feasible.**



- Parking shall not be located in front of buildings that face the street. This will also enhance the pedestrian-oriented character of the area by minimizing the amount of parking that is exposed to the edges of public sidewalks.

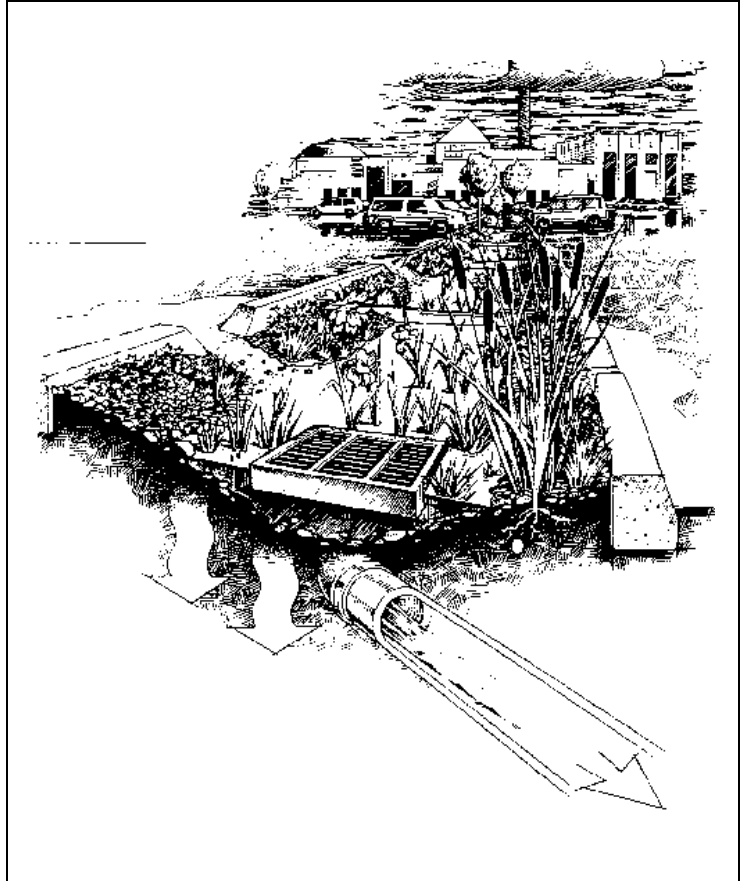
*Storm Drainage*

**85. Design drainage areas such that they will also serve as site amenities.**

- Landscape storm water detention areas, for example.
- Use open drainage ditches and line them with grasses and other appropriate plantings rather than installing underground pipes or constructing concrete channels.

**86. Design site drainage such that it does not impact adjoining properties.**

- Storm run-off may not drain onto adjacent properties.
- Drain surface water to collection points such that it can be introduced into a municipal storm sewer system.
- Storm drainage may not negatively affect adjacent wetland and natural areas. Use native plant materials to scrub storm runoff in detention areas.
- Use open drainage ditches and line them with grasses and other appropriate plantings rather than installing underground pipes or constructing concrete channels.



*Landscaping*

*In the Shoreline Resort District, the minimum lot size is 10,000 square feet and the maximum lot coverage of buildings is 75 percent. As a result of the low lot coverage, these lands can develop to their zoned intensity while also preserving a sense of open space, if paved areas are minimized and natural landscaping is emphasized. This means that significant areas of open site development will occur that include parking, landscaping, recreation sites and service areas. All of these areas should be designed to visually enhance the area.*

**87. The portion of the property not covered by structures shall be landscaped.**

- This includes parking areas, setbacks, and utility installations.
- Restore areas that are disturbed during construction with landscaping.

**BUILDING DESIGN ISSUES**

*Building Setbacks*

*No building setbacks are required, except where a development abuts a residential zone. Setbacks are likely, however, at least along side and rear lot lines because of the 50% lot coverage ratio. where traditional Old Town commercial buildings are to be developed,*

*locating the facade, or primary elevation, of the building at the sidewalk edge, with no front setback, is preferred.*

- 88. Where traditional commercial buildings are to be developed, locate the facade (primary elevation) of the building at the sidewalk edge.**
- This will help to create a pedestrian- oriented sidewalk zone.
- 89. Where coastal resort style buildings are to be used, front setbacks are appropriate.**
- The front setback is typically established from the primary street. Where clustered developments occur and buildings do not abut a street directly, orienting the facade to prominent views or open space is appropriate.

*Where traditional commercial buildings are to be developed, locate the facade (primary elevation) of the building at sidewalk edge.*

- 90. Locating parking areas in the front setback is discouraged.**
- Locate parking to the side and to the rear instead.
- 91. Combining side and rear yard setbacks along shared property lines is encouraged.**
- This will help to maximize the sense of open space.
  - It will also provide an opportunity for greater landscaped areas.

#### *Building height*

*The building should appear to fit in with the natural setting; therefore it should be low in profile.*

- 92. Minimize the perceived height of buildings in the Shoreline Resort District.**
- Each building should appear no taller than a three-story structure, with any fourth floor area contained in the roof forms. This will reduce the perceived scale of the structure.
  - Sloping roof forms should be used to reduce the profile of buildings.
  - Building forms should appear to step down in scale.
- 93. The maximum height of a building is defined in the zoning ordinance.**
- The height of a sloping roof is measured to the top of the ridge.
  - The building height is also limited to four floors, including the basement.
  - Note that this height limit is the maximum, not the suggested norm. Buildings that are less than 55 feet in height are encouraged.

#### *Scale and Proportion*

- 94. A new building should appear to fit in with the natural setting.**
- It should appear low to the ground, low in profile.

- 95. A building should have a sense of pedestrian scale.**
- Use small scale elements at the ground level, including windows and ornamental details, that will provide interest to pedestrians.
  - In general, avoid long segments of blank walls that would discourage pedestrian activity.
  - Building materials, windows and doors should be in sizes that are familiar such that pedestrians can establish a sense of scale for the structure.
- 96. A large building should have a horizontal emphasis in its overall proportion.**
- This horizontal emphasis should be balanced with some vertical elements.
  - Subdivide the building mass into subordinate forms that have a sense of horizontal emphasis.
  - Use fenestration patterns to provide contrasting vertical elements.

*A large building should have a horizontal emphasis in its overall proportion.*

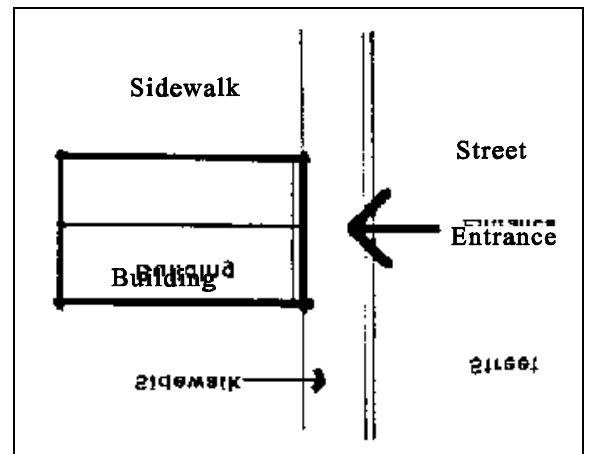
## TEST LEVEL 2

### DESIGN GUIDELINES FOR THE SHORELINE RESORT AND SHORELINE RESORT RESTRICTED DISTRICTS

#### SITE DESIGN ISSUES

##### *Directional expression and building orientation*

- 97. Primary entrances should be oriented to major pedestrian routes.**
- Buildings should appear to "address" the street. This will also help establish a sense of a pedestrian-friendly neighborhood.
  - When a building is aligned with and near the sidewalk edge, provide an entrance facing the street.
  - When oriented to the interior, locate entrances facing pedestrian paths that are internal to the development.
- 98. A building that is located along a street should have its primary entrance oriented to the sidewalk.**
- Building faces should provide visual interest.
  - Blank walls along sidewalks are discouraged.



**99. A building located in the interior of a lot may orient to ocean views or to interior landscaping.**

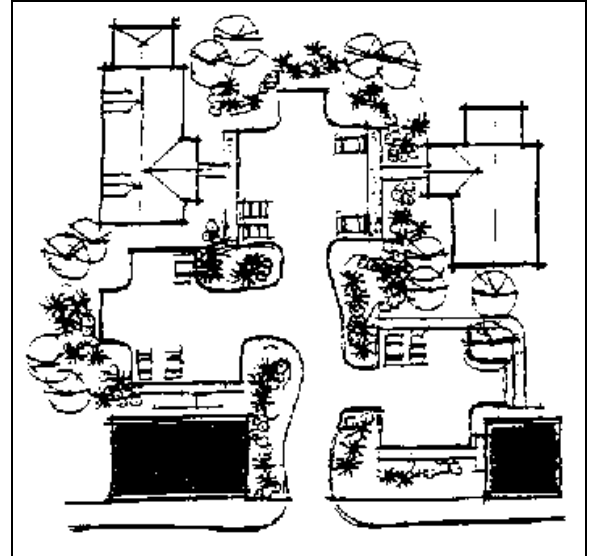
- Orienting such buildings to look onto adjacent parks and open spaces is also appropriate and encouraged.

*Arrangement of parking lots*

(Also see the parking guideline on page xx.)

**100. Divide large parking lots into smaller areas which are separated by landscaping.**

- This will reduce the perceived scale of the parking areas.
- Encourage one landscaped island of a minimum of 400 square feet to be located in the interior of a large parking lot, for every 12 cars.



**102. Minimize the visual impacts of parking areas as seen from public ways.**

- Keep the elevation of parking areas low. Avoid filling that would elevate parking areas and thereby make them more visible, even if the building itself is elevated.



**103. The use of porous paving materials for parking areas is encouraged.**

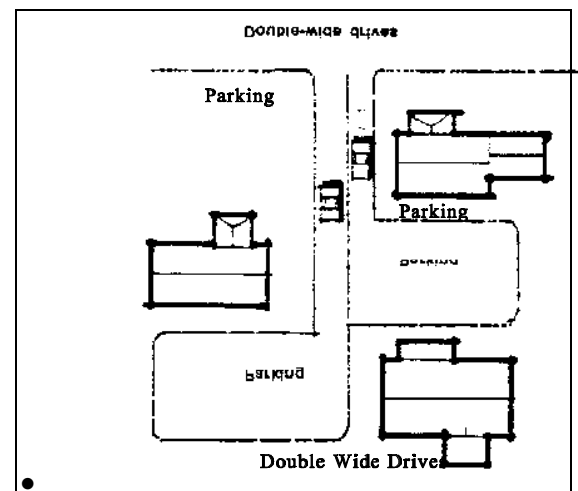
- Porous materials will help to reduce storm run-off and minimize the visual impact of parking.
- Provide appropriately smooth, hard and structurally sound surfaces, however, where accessibility for the disabled is planned.

**104. Screen the edges of parking lots.**

- Use landscaping to define the edges of parking lots that abut the public way.
- Fences, hedges and berms are options for screens that may be considered. These features will in turn enhance the visual interest of the area for pedestrians.

**105. Minimize the number and the length of curb cuts along streets.**

- Consider using shared, or paired, driveways to minimize curb cuts. This may also provide other side yards that

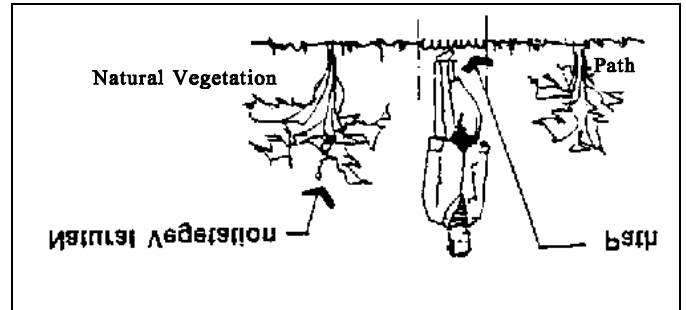


can be more effectively developed as landscaped amenities rather than driveways.

- Cross-property easements may be considered to establish shared driveways.
- In all cases, adequate emergency vehicle and service access should be provided.

### *Trails and walkways*

*A goal for the Shoreline Resort District is to promote the development of pedestrian trails that will connect properties throughout the area. Providing links to the city board walk is also desired; therefore, any development in the Shoreline Resort District should include walkways that will serve as a part of this trail system.*



#### **106. Retain a natural trail image through a development.**

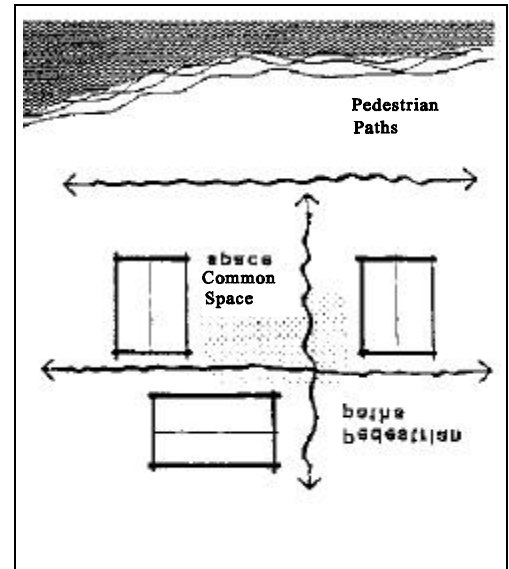
- Provide paths through properties that will connect to natural features and amenities in the area.
- These trails may be located along property lines in the required setback areas.

#### **107. Trails should be provided that will connect with paths on adjoining properties.**

- Connections to trails in north-south directions are particularly important. Also provide trail connections to the city boardwalk and the beach itself, where feasible.

#### **108. Use paving materials that will retain a rustic, natural image.**

- Primary trails should be paved in such a manner as to be accessible to disabled users.
- If concrete is used, consider the use of colors that will blend with the sand dunes.
- If asphalt is used, it should be colored in muted earth tones.
- Wooden boardwalks and trails paved with gravel, packed sand or crushed shells are appropriate designs for secondary routes.



## **BUILDING DESIGN ISSUES**



### *Roof form and pitch*

**109. The primary roof of a building shall be a sloping form.**

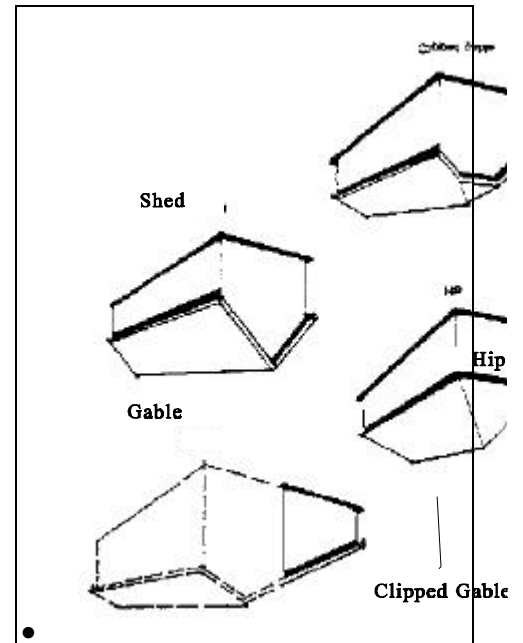
- Gabled, hipped or shed roofs are appropriate.
- A flat roof may not be the dominant roof of a building.
- The amount of flat roofs should be minimized; no more than 30% of the roof area in the plan may be a flat roof.

**110. Steeply pitched roofs are preferred.**

- Gabled and shed roofs should have a minimum slope of 5:12.

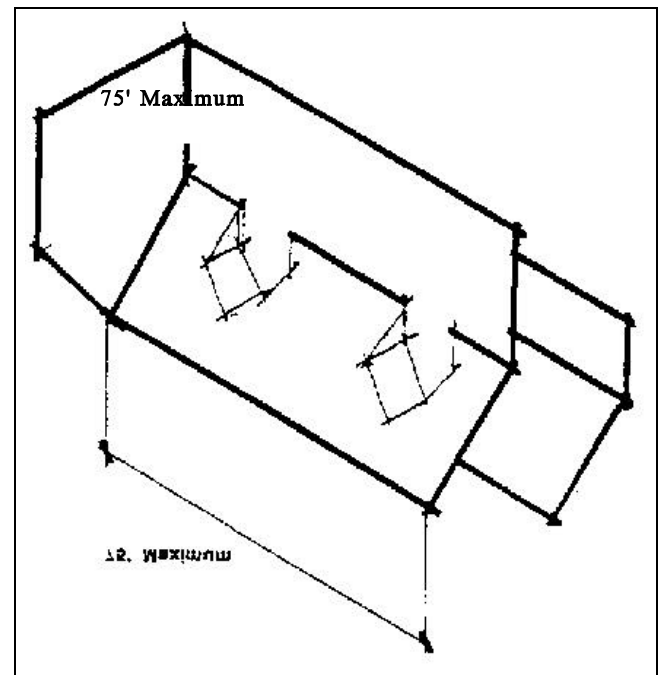
**111. Minimize the sense of scale of roof forms.**

- The uninterrupted ridge line of a roof shall not exceed 50 feet.
- Break up longer roof forms by varying ridge heights and stepping down building elements.
- Also use dormers to break up the perceived size of roofs.



**112. Minimize the visual impact of mechanical equipment mounted on buildings.**

- Mechanical equipment shall be concealed in roof forms. Incorporating equipment in gabled roofs is preferred.
- In general, no exposed mechanical equipment is allowed.
- Where it is not possible to conceal equipment in the roof, a screen wall shall be used. Exterior screen walls shall not exceed six feet in height.



### *Roof materials*

**113. Composite roofing shingles are encouraged.**

- Tan or earth-tone colored composition shingle roofing is also allowed.
- Wood shingles and tiles (again, in muted earth tones) are also appropriate roofing materials.

### *Building Siding*

**114. The primary exterior siding material should be wood.**

- Natural wood siding, such as board and batten, horizontal clapboard, shiplap, or wood shingles, that are applied to the building exterior, are appropriate.
- Wood shingles are especially encouraged in the Shoreline Resort District.
- Matte and semi-gloss finishes are appropriate; avoid high gloss surfaces.

*Doors*

**115. Wood doors are preferred.**

- Doors that are similar in character to wood doors may also be considered.

*Windows*

**116. Wood windows are preferred.**

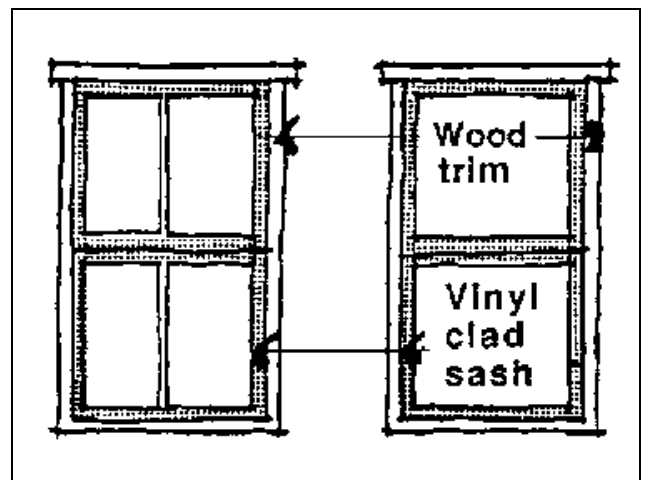
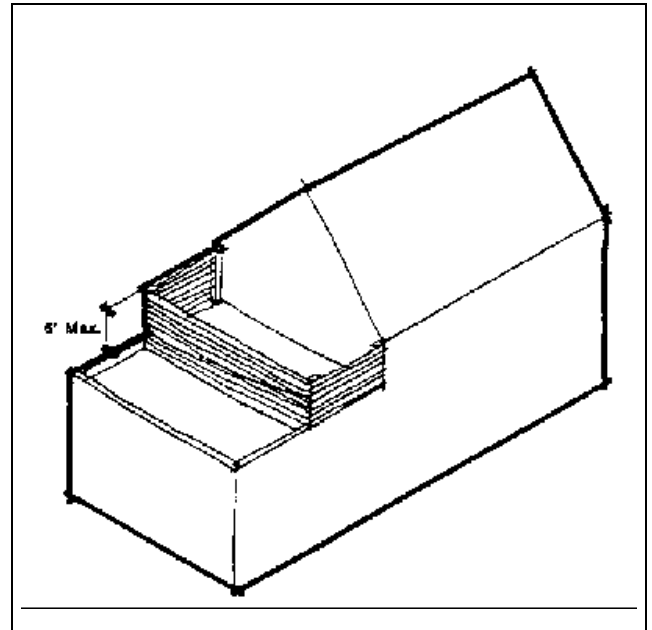
- Vinyl sash is acceptable if it is trimmed with an exterior wooden border.
- If synthetic materials are to be used, they must have a demonstrated durability.
- The window sash must have a matte, or semi-gloss painted finish.

**117. Windows with vertical proportions are preferred.**

- This is especially appropriate on upper stories.
- These may be combined in sets to create larger areas of glass.

**118. For commercial properties facing the street, provide traditional storefront display windows.**

- These provide visual interest to pedestrians and contribute to a sense of a pedestrian oriented environment.



**TEST LEVEL 3 DESIGN GUIDELINES  
FOR THE SHORELINE RESORT DISTRICT AND SHORELINE RESORT  
RESTRICTED DISTRICT**

**SITE DESIGN ISSUES**

*Pedestrian orientation*

**119. Plan developments in the Shoreline Resort District such that they invite pedestrian activity.**

- Design building faces to contain features, such as display windows or wall decorations, that will be of interest to pedestrians.
- Develop walkways that link properties together and attract pedestrian use.

*Plant materials*

**120. Use plant materials that are common to the area.**

- Native species and others that are now well-established locally are encouraged.
- Use of these similar materials will help to achieve a sense of visual continuity.

**121. Maintain natural seashore vegetation in site development wherever feasible.**

- Preservation of existing dune grasses is encouraged.
- Preservation of natural vegetation along the edges of preserved open space areas (such as the State-owned lands) is also important.

**122. The use of native vegetation is also encouraged in new landscape designs.**

- Installation of new dune grasses in the landscape design is strongly suggested.

*Maintain natural seashore vegetation in site development wherever feasible.*

*Land forms*

**123. The use of berms, similar in character to sand dunes, is encouraged in landscape designs.**

- Berms are especially appropriate to screen parking and service areas and to reduce the perceived scale of buildings.

*Site decorations*

*Some commercial sites include decorative elements such as outdoor sculpture. Many of these features, such as buoys and floats, have nautical themes. These elements are allowed, but should appear subordinate to the overall project design and the site landscaping.*

**124. Locate site decorations within the context of landscaped areas.**

- Site decorations should accurately convey the heritage of the community.
- Symbols and product forms that convey the nature of the business conducted are also appropriate.

*Screening of amusement sites*

**125. Provide landscaping that will screen the edges of amusement sites from view as seen from the public way.**

- Use fences, hedges and landscape devices to define the edges of sites along public sidewalks. These features will also enhance safety, and provide interest to

pedestrians.

## **BUILDING DESIGN ISSUES**

### *Porches and balconies*

**126. A primary entrance to a building shall be defined and protected by a porch or marquee.**

- A porch should be one story in height.
- This will enhance the pedestrian orientation of the development and reduce the perceived scale of the building.

*The use of berms, similar in character to sand dunes, is encouraged in landscape designs.*

*Primary entrances to buildings shall be defined and protected by a porch or marquee.*

**127. The use of balconies is encouraged.**

- Consider using balconies to break up the perceived scale of building elevations.

**128. Minimize the visual impact of decks as seen from the public way.**

- Screen decks from view with buildings and landscaping.
- Raised decks that will be visible from the public way are discouraged.

### *Fences*

**129. The use of fences is encouraged to define the edges of open areas.**

- Decorative fences are preferred, particularly wooden picket fences.
- Metal fences that are simple in character also may be considered.

### *Expression of detailing*

**130. The use of ornamental detail is encouraged.**

- Ornamental detail should not convey a false sense of history for the area.
- Ornamental detail should be located on a building such that it will provide visual interest to pedestrians.

**131. Ornamental detail should be simple.**

- Ornamental detail should appear to be integrated into the overall architectural composition.
- It should be applied to gables, eaves, and porches.
- Trim around door and window openings is also required.

*The use of fences is encouraged to define the edges of open areas. Decorative fences are preferred, particularly wooden picket fences.*

### *Colors*

**132. Natural colors, as found in land forms and vegetation in the Shoreline Resort Zone, are encouraged.**

- Muted colors that blend with the colors of natural features in the area are appropriate.
- Bright colors that contrast strongly with the natural colors of the area are inappropriate.
- Matte and semi-gloss finishes are appropriate for building exteriors. Avoid high gloss finishes.

**133. Minimize the number of colors used in a buildings's color scheme.**

- Keep color schemes simple.
- Designs using only two or three colors are encouraged.

**134. Coordinate color schemes among all the buildings in a development.**

- This will help to strengthen a sense of visual continuity among elements.

*Mechanical equipment and service areas*

**135. All ground-mounted mechanical equipment and service areas shall be screened from view.**

- Fences, berms and plantings are appropriate screening methods.

**SIGNS**

*Character of signs*

**136. Signs that are simple in design and easily read are encouraged.**

- Signs that make use of symbols are particularly encouraged.

All ground-mounted mechanical equipment and service areas shall be screened from view.

*Size of signs*

**137. The size of a sign shall not exceed the limits defined in the zoning ordinance.**

- Three square feet per dwelling unit, not to exceed 24 square feet per street frontage is allowed, per the zoning ordinance.
- The sign area shall not exceed 100 square feet for commercial signs (including both faces of two-sided signs).
- One additional sign per additional occupant, not to exceed four square feet each, is also allowed.

*Sign Location*

**138. Signs should appear to be integrated with their setting.**

- If mounted on buildings, they should appear to be a part of the architecture and fit within distinct architectural features.
- If mounted on the ground, signs should appear to be an integral part of the landscaping.
- Free-standing signs should have a low profile.

**139. Signs should appear to be in proportion to their building.**

*Sign materials*

**140. Signs shall have materials and colors that are similar to those of the building.**

*Illumination of signs*

**141. Signs may only be lighted by concealed, indirect lighting.**

- No flashing lights are permitted.
- No internal illumination is permitted.

Signs may only be lighted by indirect sources. These lamp shades conceal the light source and control glare.

**OTHER**

*Cohesiveness*

**142. Building and site designs shall be devised such that a development has a sense of being visually integrated.**

- Use similar building forms, materials and colors, throughout an individual development such that a sense of visual continuity is achieved.
- Also use similar landscape elements throughout the development to reinforce a sense of being visually integrated.
- This is particularly important for large developments where several buildings are to be constructed as a part of an individual project.

*General spirit of the project*

**143. The development shall convey a sense of a being "new coastal development" that blends with the seashore setting and respects the natural resources that make up the character of this zone.**

- The existing sand dune character is to be retained.
- The natural seashore vegetation shall be maintained.

The development should be distinguishable from Old Towne in its architectural character, in that the seashore area should be recognized as being an area of more recent development.

## SHORELINE MULTIFAMILY

### TEST LEVEL 1 GUIDELINES FOR THE SHORELINE MULTIFAMILY DISTRICT

#### SITE DESIGN ISSUES

##### *Lot size*

**144. The minimum lot size shall comply with the zoning ordinance.**

- The minimum lot size is 6,000 square feet for single family dwellings.
- Multi-family lot minimums shall be 6,000 square feet for the first dwelling plus 2,000 square feet for each additional dwelling unit.

*Buildings should appear to be settled into the sand dunes.*

##### *Siting*

**145. Locate buildings on their sites such that they appear to be settled into the sand dunes.**

- Also use berms and other sloping land forms against buildings to reduce their perceived building height.
- Retain the existing topography where feasible and minimize filling.
- Also establish a natural transition in site grades to neighbors properties.

##### *Position of the building on site; view protection and enhancement*

**146. Locate a building on its site such that views through the development are retained and the amount of perceived open space is maximized.**

- Where feasible, views from the public way, including from streets, and the beach (in particular), should be preserved.
- Locate a building on its site such that side and rear yards may be combined visually with adjacent properties, in order to increase the visual impact of open space.

*Locate a building on its site such that views through the development are retained and the amount of perceived open space is maximized.*

##### *Location of parking areas* (also See parking guidelines)

*The visual impact of parking should be minimized throughout the area. The intent is to minimize the amount of paved area in development, retain a natural image and reduce storm run-off.*

**147. Parking should be located to the rear, or interior, of lots wherever feasible.**

- Parking shall not be located in front of buildings that face the street

*Storm Drainage*

**148. Design drainage areas such that they will also serve as site amenities.**

- Landscape storm water detention areas, for example.

**149. Design site drainage such that it does not impact adjoining properties.**

- Drain surface water to collection points such that it can be introduced into a municipal storm sewer system.
- Use native plant materials to scrub storm runoff in detention areas.
- Storm drainage may not negatively affect adjacent wetlands and natural areas.

**BUILDING DESIGN ISSUES**

*Setbacks*

*Parking should be located to the rear, or interior, of lots wherever feasible. Combining driveways with adjacent properties is also encouraged.*

**149. Setbacks shall comply with the standards defined in the zoning ordinance.**

- Setbacks greater than the minimums allowed are encouraged.
- The minimum front yard setback shall be ten feet.
- The minimum side yard setback shall be five feet
- The minimum rear yard setback shall be ten feet.

*Use native grasses in drainage ways where feasible.*

**150. Locating parking areas in the front setback is discouraged.**

- Locate parking to the side and to the rear where feasible.

**151. Combining side and rear yard setbacks along shared property lines is encouraged.**

- This will help to maximize the sense of open space.
- It will also provide an opportunity for larger landscaped areas.

*Building height*

**152. Building heights may not exceed those limits established in the zoning ordinance.**

- The maximum height is 35 feet, measured from the average grade to the highest point on the roof.

*Minimize the perceived height of building in the Shoreline Multi-family Residential District.*



**153. Minimize the perceived height of buildings in the Shoreline Multi-family Residential District.**

- Sloping roof forms should be used to reduce the profile of buildings.
- Each building should appear no taller than a two-story structure, with the third floor area contained in the roof forms. This will reduce the perceived scale of the structure.
- Building forms should appear to step down in scale.

*Scale and proportion*

**154. New buildings should appear to fit in with the natural setting.**

- Buildings should appear low to the ground, low in profile.

New buildings should appear to fit in with the natural setting.

**155. Buildings should have a sense of pedestrian scale.**

- Small scale elements shall be used at the ground level that provide visual interest to pedestrians.
- Use windows and doors in sizes seen traditionally, such that the viewer can relate to the scale of these elements.
- Avoid long segments of blank walls that would discourage pedestrian activity.

**156. A large building should have a horizontal emphasis in its overall proportion.**

- This horizontal emphasis should be balanced with some vertical elements.
- Break up the building mass into subordinate forms that have a sense of horizontal emphasis.
- Use fenestration patterns to provide contrasting vertical elements.

Small scale elements should be used at the ground level that provide visual interest to pedestrians.

Large buildings should have horizontal emphasis in their overall proportion. Dividing buildings into smaller components helps reduce the perceived scale of the structure.

**TEST LEVEL 2 DESIGN GUIDELINES  
FOR THE MULTI-FAMILY DISTRICT**

**SITE DESIGN ISSUES**

*Directional expression and building orientation*

**157. Primary entrances should be oriented to major pedestrian routes.**

- When aligned at the sidewalk, provide an entrance facing the street.
- When oriented to the interior, locate entrances facing pedestrian paths.

**158. A building that is located along a street edge should have its primary entrance oriented to the sidewalk.**

- Again, blank walls along sidewalks are inappropriate because building faces should provide visual interest to the pedestrian.

*A building that is located at the street should have its primary entrance oriented to the sidewalk.*

**159. A building that is located in the interior of a lot may orient to ocean views or to interior landscaping.**

- Orienting such buildings to look onto adjacent parks and open spaces is also appropriate, and is encouraged.

*Arrangement of parking lots*

(Also see the parking guidelines)

**160. Divide large parking lots into smaller areas separated by landscaping.**

- This will reduce the perceived scale of the parking areas.
- Provide one landscaped island of a minimum of 400 square feet in the interior of a parking lot for every 12 cars.



*Divide large parking lots into smaller areas separated by landscaping.*

- 161. Minimize the visual impacts of parking areas as seen from public ways.**
- Keep the elevation of parking areas low. Avoid filling sites that would elevate parking areas and thereby make them more visible, even if the building itself is elevated.
- 162. The use of porous paving materials for parking areas is encouraged.**
- Porous materials will help to reduce storm runoff and minimize the visual impact of parking.
  - Provide appropriate smooth, hard surfaces, however, where accessibility for the disabled is planned.
- 163. Screen the edges of parking lots.**
- Use landscaping to define the edges of parking lots along the public way.
  - Fences, hedges and berms are options that may be considered.
- 164. Minimize the number and extent of curb cuts along the street.**
- Use of shared, or paired, driveways is encouraged, as a means of creating other side yards that can be more effectively developed as amenities.
  - Cross-property easements may be considered to establish shared driveways where adequate safety and service access is provided.
  - In all cases, adequate emergency vehicle and service access should be provided.

#### *Trails and walkways*

*A goal for the Shoreline Resort District is to promote the development of pedestrian trails that will connect properties throughout the area; providing links to the city boardwalk is also desired. Any development in the Shoreline Multi-Family District should include walkways that will serve as a part of this trail system.*

- 165. Retain a natural trail image through a development.**
- Provide paths through properties.
  - These may be located along property lines, in the setback areas.

*Trails must be provided that will connect with paths on adjoining properties.*

- 166. Trails must be provided that will connect with paths on adjoining properties.**
- Connections to trails in north-south directions are particularly important
  - Also provide connection to the city boardwalk and the beach.
- 167. Use paving materials that will retain a rustic, natural image.**
- Primary trails should be paved in such a manner as to be accessible to disabled users.
  - If concrete is used, consider the use of colors that will blend with the sand dunes.
  - If asphalt is used, it should be coated in a muted earth tone color.
  - Wooden boardwalks, and trails paved with gravel, packed sand or crushed shells are appropriate designs for secondary routes.

Use paving materials that will retain a rustic, natural image.

## **BUILDING DESIGN ISSUES**

### *Roof form and pitch*

**168. The primary roof of a building shall have a sloping form.**

- Gabled, hipped or shed roofs are appropriate forms.
- A flat roof may not be the dominant roof of a building.
- The amount of flat roofs should be minimized; in general, no more than 30% of the roof area in plan should be a flat roof.

**169. Steeply pitched roofs are preferred.**

- Gabled and shed roofs shall have a minimum slope of 6:12.

Gabled, hipped or shed roofs are appropriate forms.

Minimize the perceived scale of the roof.

**170. Minimize the perceived scale of the roof.**

- The uninterrupted ridge line of a roof shall not exceed 75 feet.
- Break up longer roof forms by varying ridge heights and stepping down building elements. This will reduce its apparent size.
- Also use dormers to break up the perceived size of the roof.

**171. Minimize the visual impact of mechanical equipment mounted on buildings.**

- Mechanical equipment shall be concealed in roof forms. Incorporating equipment in gabled roofs is preferred.
- In general, no exposed mechanical equipment is allowed.
- Where it is not possible to conceal equipment in the roof, a screen wall shall be used. Exterior screen walls shall not exceed six feet in height.

### *Roof materials*

**172. Composite roofing shingles are encouraged.**

- Tan or earth-tone colored composition shingle roofing is also allowed.
- Also acceptable are wood shingles and roof tiles (again, in muted earth tones).

### *Building Siding*

**174. The primary exterior siding material should be wood.**

- Natural wood siding, such as board and batten, horizontal clapboard, shiplap, or wood shingles are appropriate siding materials.
- Wood shingles are especially encouraged for siding in the Shoreline Multi-Family District
- Matte and semi-gloss finishes are appropriate; shiny surfaces are to be avoided for

primary exterior siding materials.

Wood clapboard siding is an appropriate material.

## Windows

**175. Wood windows are preferred.**

- Vinyl sash is acceptable if it is trimmed with an exterior wooden border.
- If synthetic materials are to be used, they must have a demonstrated durability.
- The window sash must have a matte or semi-gloss painted finish.

**176. Windows with vertical proportions are preferred.**

- This is especially appropriate on upper stories.
- These may be combined in sets to create larger areas of glass.

**177. For commercial properties facing the street, provide traditional storefront display windows.**

- These provide visual interest to pedestrians and contribute to a sense of a pedestrian-oriented environment.

### Doors

Although an all-wood window is preferred, vinyl sash may be considered if it is trimmed with a wooden border.

**178. Wood doors are preferred.**

- Materials that appear similar in character to wood may also be considered for doors.

Wood windows are preferred. Breaking up roof forms into subordinate elements helps reduce the perceived scale of this building. The fence provides visual interest to pedestrians.

## TEST LEVEL 3 DESIGN GUIDELINES FOR THE SHORELINE MULTI- FAMILY DISTRICT (S2)

## **SITE DESIGN ISSUES**

### *Pedestrian orientation*

**179. Plan each development such that it will encourage pedestrian activity throughout the neighborhood.**

- Develop walkways that link properties together, and that are attractive to pedestrians as well.

### *Plant materials*

**180. The use of native and traditional plant materials throughout the area is encouraged.**

- This will help to achieve a sense of visual continuity.
- Installation of new dune grasses and shore pines in landscape design is strongly suggested.

**181. Maintain natural seashore vegetation in site development wherever feasible.**

- Preservation of existing dune grasses and shore pines is encouraged.
- Preservation of natural vegetation along the edges of preserved open space areas (such as the State owned lands) is especially important.

*Maintain natural seashore vegetation in site development, wherever feasible.*

### *Land forms*

**182 The use of berms, similar in character to sand dunes, is encouraged in landscape designs.**

- Berms are especially appropriate to screen parking and service areas and to reduce the perceived scale of buildings.

### *Site decorations*

*Some sites include the use of decorative elements, such as outdoor sculpture. Many of these features have nautical themes, such as buoys and floats. These elements are allowed, but should be subordinate to the overall project design and the site landscaping.*

**183. Locate site decorations within the context of landscaped areas.**

- Site decorations should accurately convey the heritage of the community.

*Locate site decorations in landscaped areas.*

## **BUILDING DESIGN ISSUES**

### *Porches and balconies*

**184.. A primary entrance to a building should be defined and protected by a porch or marquee.**

- Porches should be one story in height. These enhance pedestrian orientation and reduce the perceived building scale.

**185. The use of balconies is encouraged.**

- Consider using balconies to break up the perceived scale of building elevations.

**186. Minimize the visual impact of decks as seen from the public way.**

- Screen decks with buildings and landscaping.
- Raised decks are discouraged.

*Primary entrances shall be defined and protected by a porch or marquee.*

*Expression of detailing*

**187. The use of ornamental detail is encouraged.**

- Ornamental detail should not convey a false sense of history for the area.
- Detail should be located such that they provide visual interest to pedestrians.

**188. Ornamental detail should be simple in design.**

- Ornamental detail should appear to be integrated into the overall architectural composition.
- It should be applied to gables, eaves and porches.
- Trim around door and window openings is also appropriate.

*Color*

**189. Natural colors, as found in land forms and vegetation in the Shoreline Multi-Family District, shall be used.**

- Muted colors that blend with the colors of natural features in the area are appropriate.
- Bright colors that contrast strongly with the natural colors of the area are inappropriate.
- Matte and semi-gloss finishes are appropriate for building exteriors. Avoid high gloss finishes.

**190. Minimize the number of colors used in a building's color scheme.**

- Keep color schemes simple. Designs using only two or three colors are encouraged.

**191. Coordinate color schemes among all the buildings in a development project.**

- This will help to strengthen a sense of visual continuity among the project elements.

Fences are encouraged.

*Fences*

**192. The use of fences is encouraged to define the edges of open areas.**

- Decorative fences are preferred, particularly wooden picket fences.
- Metal fences which are simple in character also may be considered.
- Chain link and split rail wood fences are not encouraged.

*Mechanical equipment and service areas*

All ground-mounted mechanical equipment and service areas shall be screened from view.

**193. All ground-mounted mechanical equipment and service areas shall be screened from view.**

- Fences, berms and plantings are appropriate screening methods.

**OTHER**

*General spirit of the project*

**194. The development shall convey a sense of a "new coastal development" that blends with the seashore setting and respects the natural resources that make up the character of this zone.**

- The existing sand dune character is to be retained.
- The project shall appear to be settled into the sand dunes.
- The natural seashore vegetation shall be maintained.
- The development should be distinguishable from Old Town in its architectural character, in that the seashore area should be recognized as being a more recent development.

*Cohesiveness*

**195. All buildings in a development project, and their related site shall be designed such that the entire development has a sense of visual integration.**

- Similar forms, materials, colors, landscaping, scale, etc. shall be used throughout such that a sense of visual relatedness with adjacent sites is achieved.

**SINGLE FAMILY RESIDENTIAL RESTRICTED**

**TEST LEVEL 1 GUIDELINES**



**FOR THE SINGLE FAMILY RESIDENTIAL RESTRICTED (RI R) DISTRICT**

**SITE DESIGN ISSUES**

*Lot size*

- 196. Buildable lot sizes shall conform to the minimum dimensions defined in the zoning ordinance.**
- The minimum lot size is 6,000 square feet for single family dwellings.

*Lot coverage*

- 197. The maximum lot coverage for single family dwellings shall not exceed 60% of the lot area.**
- The remainder of the site shall be landscaped.

*Storm Drainage*

- 198. Design site drainage such that it does not impact adjoining properties.**
- Drain surface water to collection points such that it can be introduced into a municipal storm sewer system.

**BUILDING DESIGN ISSUES**

Maximum lot coverage shall not exceed 60%

*Setbacks*

- 199. Setbacks shall comply with the standards defined in the zoning ordinance.**
- Setbacks greater than the minimums allowed are encouraged.
  - The minimum front yard setback shall be ten feet.
  - The minimum side yard setback shall be five feet.
  - The minimum rear yard setback shall be ten feet.

*Building height*

- 200. Buildings shall reinforce the perceived scale of older dwellings in the neighborhood.**
- The maximum building height shall be 35 feet, measured from the average grade to the highest point of the roof.
  - Buildings that have an appearance of one or two stories are preferred.

**Scale and proportion**

- 201. Buildings should have a sense of pedestrian scale.**

- Small scale elements shall be provided at the ground level that provide visual interest to pedestrians.
- Use materials that occur in modules that have recognizable dimension that are familiar to pedestrians such that they can determine the scale of the building.
- Use windows and doors in sizes seen traditionally, such that the viewer can relate to the scale of these elements.
- Avoid long segments of blank walls that would discourage pedestrian activity.

*The maximum building height shall be 35 feet, measured from the average grade to the highest point of the roof.*

## **TEST LEVEL 2 DESIGN GUIDELINES FOR THE SINGLE FAMILY RESIDENTIAL RESTRICTED (RI R) DISTRICT**

### **SITE DESIGN ISSUES**

#### *Orientation*

#### **202. Buildings shall orient to the street.**

- Primary entrances should face the street.

#### *Arrangement of parking areas*

#### **203. Locate parking areas such that their visual impact is minimized, as seen from the street.**

- Locate parking to the side or rear of primary structures, where feasible.

#### **204. Consider screening parking areas from street view.**

- Use fences and hedges to screen parking.

#### **205. The use of porous paving materials is encouraged, to reduce storm water runoff.**

- Concrete or other smooth surfaces are appropriate where accessibility for persons with mobility impairments is needed.

### **BUILDING DESIGN ISSUES**

#### *Building Siding*

#### **206. Natural wood siding, such as board and batten, clapboard, shiplap, and wood shingle, is encouraged.**

- Synthetic materials, such as vinyl siding may be considered, if it can be demonstrated that their appearance will be similar to wood, over an extended period.
- Matte finishes or semi-gloss are preferred; avoid shiny surfaces.

*Locate parking to the side or rear of primary structures, where feasible.*

*Natural wood siding, such as board and batten, clapboard, shiplap and wood shingle, is encouraged.*

*Roof materials*

**207. Composite roofing shingles are encouraged.**

- Tan and earth-tone colored composition shingles are also allowed.
- Also acceptable are wood shingles and roof tiles (again, in muted earth tones). Shingle patterns and textures that are similar in character to that of traditional sawn wood shingles are preferred.

*Windows*

**208. Wood windows are preferred.**

- Vinyl and metal sash are acceptable materials if they are trimmed with an exterior wooden border of at least a 4" nominal width.
- If synthetic materials are to be used, they must have a demonstrated durability.
- The window sash must have a matte or semi-gloss painted finish.

**209. Windows with vertical proportions are preferred.**

- Traditional double-hung windows are preferred.
- This is especially appropriate on upper stories.
- These may be combined in sets to create larger areas of glass.

*Roof form and pitch*

**210. The dominant roof of a primary dwelling shall be a gable form.**

- Gabled, hipped or shed roofs are appropriate for subordinate roofs and secondary structures.
- Simple roofs of one form may be considered, and moderately complex roof compositions, made up of a series of two or three intersecting roof forms are also appropriate.

*Vinyl and metal sash are acceptable materials on a window if they are trimmed with an exterior wooden border of at least a 4" nominal width.*

**211. Steeply-pitched roofs are preferred.**

- Gabled and shed roofs shall have a minimum slope of 6:12.

*Gabled, hipped or shed roofs are appropriate.*

**TEST LEVEL 3 DESIGN GUIDELINES OR THE SINGLE FAMILY  
RESIDENTIAL RESTRICTED (RI R) DISTRICT**

## **BUILDING DESIGN ISSUES**

### *Porches*

**212. Porches shall be provided.**

- These enhance pedestrian orientation and reduce perceived building scale. Porches that are one story in height are particularly appropriate because they help to establish a sense of pedestrian scale.
- Porches should face the street.
- At least 50% of the porch should remain open, as seen from the street.

### *Fences*

**213. The use of fences is encouraged to define the edges of open areas.**

- Decorative fences are preferred, particularly wooden picket fences.
- Metal fences which are simple in character also may be considered.
- Chain link and split rail wood fences are discouraged.

### *Expression of detailing*

**214. The use of ornamental detail is encouraged.**

- Ornamental detail should not convey a false sense of history for the area.
- Details should be located such that they provide visual interest to pedestrians.

**215. Ornamental detail should be simple in design.**

- Ornamental detail should appear to be integrated into the overall architectural composition.
- It should be applied to gables, eaves and porches.
- Trim around door and window openings is also appropriate.

### *Color*

**216. Natural colors, as found traditionally in the area, are encouraged.**

- Muted colors that blend with the colors of natural features in the area are appropriate.
- Bright colors that contrast strongly with the natural colors of the area are discouraged.
- Matte and semi-gloss finishes are appropriate for building exteriors. Avoid high gloss finishes.

**217. Minimize the number of colors used in a color scheme.**

- Keep color schemes simple.
- Designs using only two or three colors are encouraged.

**218. Coordinate the color schemes among all the buildings in a development project.**

- This will help to strengthen a sense of visual continuity among elements.

*Mechanical equipment*

**219. Minimize the visual impact of mechanical equipment.**

- Enclose mechanical equipment in structures or provide a screen where feasible.
- Locate antennas and satellite dishes such that their visibility from the street will be minimized.

Enclosing waste storage areas is encouraged.

Screen ground-mounted mechanical equipment.

**OTHER DESIGN ISSUES**

*Cohesiveness*

**220. The design of any new building and its site should have a sense of visual integration with established traditional residences in the neighborhood.**

- A sense of visual relatedness with adjacent sites is achieved through the use of similar forms, materials, colors, landscaping, scale, etc.

*General spirit of the project*

**221. The buildings should convey the general spirit of the single family coastal residential neighborhood that has been established traditionally.**

**222. On-site, stick built construction is preferred in the Residential Restricted Zone.**

## HOW TO DEVELOP A DESIGN PROPOSAL

The following steps are general milestones in the process and should be followed in order to complete the review and approval process in the most timely and efficient manner possible.

Applicants should also review the official application procedures as well, because details of the process may be amended from time to time. Follow these steps:

### Step 1: Review the design guidelines.

The intent of the review process is to ensure high quality design that is compatible with the city's design goals.

The design guidelines form the basis for making determinations about appropriateness, and therefore no design work should begin without reviewing the guidelines and identifying the relevant chapters, including the special neighborhood guidelines.

In all cases applicants are expected to meet as many of the guidelines as possible. In each case, the city will determine when a sufficient number of the guidelines have been adequately met in order to consider whether the design is appropriate.

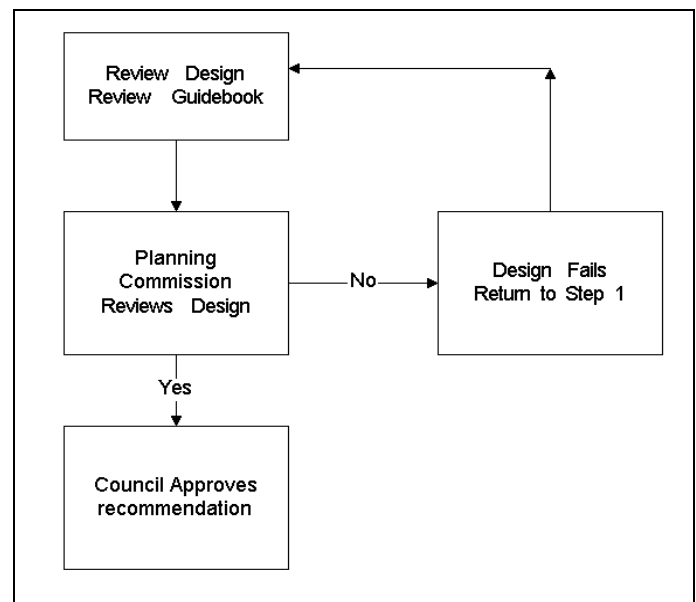
Where specific measured dimensions are used in the guidelines, the methods for taking such measurements as defined in the zoning ordinance shall be used.

### Step 2: Study the context of the building site.

The city is concerned about the visual impact of a project on its setting and, therefore, a site visit is essential. In the site analysis, one should identify those significant features, both natural and man-made, that are found in the immediate vicinity and the neighborhood at large, that define the context for the project. Many of the guidelines focus on respecting this existing context, and therefore clearly defining its character is essential.

### Step 3: Discuss the project with city staff.

Prior to filing an application to the city, the applicant or his representatives should meet



with the city staff to obtain information about the review process and the application of the guidelines. Preliminary reviews with the Planning Commission may also be important at this stage, especially on larger projects. Applicants are therefore urged to schedule an informal discussion at the earliest stage of concept development

#### **Step 4: Develop the design using the guidelines.**

Applicants should use the guidelines in this book to develop the design, since the guidelines form the basis for the city's determination of appropriateness. Applicants will be asked in formal review sessions to describe how the proposed project meets the design guidelines.

In some cases, applicants may feel that they can meet the general policies in a means other than that defined in the guidelines and may place such an argument before the commission. In such circumstances, the burden is upon the applicant to demonstrate how the proposed project meets the broader goals, objectives and policies that drive the guidelines.

#### **Step 5: Submit the design for formal review.**

Design documents must be filed in advance of formal planning commission and, where applicable, city council, meetings to allow commission members and the public at large adequate time to view the materials. Be certain that adequate information is provided and that all the pertinent details of the design are clearly presented. Many applications are delayed because the applicant does not clearly and concisely represent the project concepts. Check with city staff to determine the specific submittal requirements, however, in general, submittal documentation should include:

1. Photographs of the existing structure and/or site, showing all sides of a structure and the street where the structure is located, and
2. A site plan on a sheet 24" x 36" and drawing at a scale of 1/8" = 1' including:
  - a. the layout of the entire project and its relation to surrounding properties and existing buildings thereon;
  - b. the location of points of entry and exit of motor vehicles, the circulation pattern and parking;
  - c. preliminary building plans which show any existing structure;
  - d. type and color of all exterior materials, including samples of the materials and paint chips;
  - e. a statement describing the scope of the project, the design intentions, and the project's visual impact on its surroundings;
  - f. a map indicating the existing topography and any proposed grade changes; and
  - g. all changes to the site, including but not limited to: landscaping, walls, fences, exterior lighting, and the type and placement of signs.

**Step 6: Present the design at a public meeting.**

The Planning Commission (or City Council) will hear comments on proposed projects at its regularly scheduled meetings. Documents used at the meeting should match those filed earlier. The city shall conduct the review and approve or disapprove the application within thirty (30) days after the hearing. In making its determination, the city will use the design guidelines to determine the appropriateness of the project. The city shall submit, in writing, its decision to the Building Inspector along with the application, and if approved, a Certificate of Appropriateness.

**Step 7: Revise the design based on City comments.**

In preliminary reviews, the city may indicate areas of concern, and in later formal reviews when the commission takes action, it may determine that a project does not adequately meet the design guidelines. In both cases, applicants may modify their designs based on staff and commission comments and submit them for reconsideration.